



Public & Private Street Standards



City of Fayetteville
Engineering Division
433 Hay Street
Fayetteville, NC 28301-5537

CITY OF FAYETTEVILLE

PUBLIC STREET STANDARDS

A subdivision road is one that serves a parcel or tract of land that is subdivided into two or more lots, building site or other divisions for sale or building development for residential purposes where such subdivisions include a new road or change in an existing road.

Subdivision roads may be designated public or private. Public designation will be built to minimum construction standards of the City of Fayetteville.

Definitions

The following definitions shall apply in this manual:

1. Local residential subdivision road - Either cul-de-sacs, loop roads, roads that do not connect thoroughfare or serve major traffic generators.
 - A. Cul-de-Sacs - These are very short roads, open at one end only, with a special provision for turning around. They have a "bulb" end design with a specific turning radii and a limited number of lots.
 - B. Dead-End Roads - These are roads less than 1000 feet in length, open at one end only with special provisions for turning around and have no collector characteristics. A cul-de-sac design is the preferred choice with T-turnarounds being approved only in unusual situations.
 - C. Short Connecting Roads - These roads are normally one block long or extend on a block-by-block basis and have no collector characteristics.
 - D. Loop Roads - A road that has its beginning and ending points on the same route. It is less than one mile in length and has no collector characteristics.
 - E. Other Roads - These roads do not connect thoroughfares or serve major traffic generators and do not have "collector" characteristics.
2. Residential collector roads - A road which serves as the connecting street between local residential roads and the thoroughfare system.

- A. Dead End Roads - These roads are less than 2000 feet in length, open at one end only with special provisions for turning around, and have collector characteristics. A cul-de-sac design is the preferred choice with T-turnarounds being approved only in unusual situations.
- B. Connecting Roads - The roads which serve as the connecting road system between other roads within the subdivision and the thoroughfare system.
- C. Loop Roads - A road that has its beginning and ending points on the same route. It is more than one mile in length and has collector characteristics.
- D. Other Roads - These are other roads having "collector" type function in the thoroughfare system.

Requirements for Addition of Subdivision Roads to the System:

1. The minimum construction standards and other requirements in this manual must be a part of the proposal to be reviewed for approval in order for a plat to be recorded by the County Register of Deeds prior to development.
2. Developers or property owners must dedicate right-of-way, as indicated in the minimum design and construction criteria section of this manual, free of charge and clear of all encumbrances.
3. Utilities requiring adjustment or relocation to conform to the City of Fayetteville requirements shall be made at no expense to the City. Existing and/or relocated utilities may remain within the right-of-way of any subdivisions or added to the City System provided the location of same meeting City approval and further provided the utility owner executes an encroachment agreement with the City. Utilities are defined as electric power, telephone, television, telegraph, water, sewage, gas, oil, petroleum products, steam, chemical, drainage, irrigation and similar lines. The right of any utility placed or located on a proposed or existing subdivisions public road right-of-way shall be subordinate to the road right-of-way and the utility shall be subject to regulation by the City of Fayetteville.
4. Where extenuating circumstances exist, the City Engineer and the Traffic Engineer have the authority to allow deviations from the design and construction criteria presented in this manual. The deviations allowed will be reasonable and limited only by safety and maintenance factors.

UTILITY REQUIREMENTS

1. LOCATION

Poles and other above-ground utilities which are to remain inside the right-of-way shall be located as indicated by the City.

2. SUGGESTED DEPTH OF COVER FOR PIPE LINES AND OTHER UTILITIES

This section is under review

3. For residential subdivisions roads and residential collector roads, underground utilities may cross under or run longitudinally under the pavement.
4. Acceptable Materials for Utilities Under Existing or Proposed Pavement - Materials Not Listed Will Be Referred to the City Engineer.
 - a. Smooth Wye Steel Pipe meeting API 5L Grade B Specifications
 - b. Spiral Welded Steel Pipe meeting ASTM Specification A-211
 - c. Circular Black Steel Pipe meeting ASTM Specification A-120 or A-589
 - d. Galvanized Steel Pipe meeting ASTM A-120 Specifications
 - e. Ductile Iron Pipe - Class 50 Min. Strength
 - f. Cast Iron Pipe - Class 150 Min. Strength
 - g. Concrete Sewer Pipe - Plain and Reinforced Pipe meeting Department of Transportation Standard with Rubber Gasket Joints or equivalent Gasket Joint.
 - h. Reinforced Concrete Pressure Pipe, Steel Cylinder Type for Water and other Liquids meeting AWWA Specifications C-300, C-301 and C-303.

- i. ABS (Acrylonitrile - Butadiene - Styrene) Composite Sewer Pipe shall meet ASTM D-2680 Specifications for Pipe sizes 8" thru 15," and ASTM D-2751 for Pipe sizes 4" and 6" for Laterals. ABS Sewer Pipe shall be used for Domestic Sewage only.
- j. P.V.C. (Polyvinyl Chloride) Pipe - SDR 21, SDR 26 or C-900 for Water and SDR 35 for Sewer only.
- k. V.C. (Vitrified Clay) Sewer Pipe, Extra Strength meeting ASTM Specifications C-700 with Factory Fabricated Joints meeting ASTM Specifications C-425
- l. P.E. (Polyethylene) Plastic Pipe - SDR 7 meeting ASTM Specifications D-2239 and Plastic Tubing - SDR 9 meeting ASTM Specification D-2737 for sizes 3/4" thru 2" only.
- m. Polyethylene Plastic Pipe (High Molecular Weight) SDR 11 meeting Plastic Pipe Institute Material Designation PE 3408 in sizes up to 4.5" O.D.
- n. Type K Copper Pipe meeting ASTM Specification B-88 for sizes 3/4" thru 2"
5. Acceptable materials for utilities outside pavement shall be the same as covered in Paragraph 4 above; however, minimum strength of pipe may be reduced in accordance with applicable industry codes.
6. Any utility to be installed within the right-of-way of a city maintained road will require an encroachment permit with the City.

**MINIMUM DESIGN AND CONSTRUCTION CRITERIA
FOR SUBDIVISION ROADS**

I. CONSTRUCTION REQUIREMENTS

A. DRAINAGE

The City Engineer shall review all drainage prior to acceptance of any facility to the City Street System.

All storm drainage shall be adequate so that the road may be maintained without excessive cost, and not cause flooding on private property from storm runoff of the design frequency. Permanent drainage easements may be required. The minimum design frequency shall be as follows but may be increased at the recommendation of the City Engineer.

1. Storm sewer collector - 10 years
2. Cross Drainage - 10 years

B. BRIDGES AND DAMS

Bridges

Minimum criteria for bridges to be built by private interest for future acceptance by the City.

1. Structures which are to span streams shall be designed for hydraulic requirements in accordance with City of Fayetteville criteria, and plans shall be submitted to the City Engineer for review and approval.

Rip Rap will be required as recommended by the City Engineer

2. Structures shall be designed for minimum live load of HS-20 as specified in the Standard Specifications for Highway Bridges of the American Association of State Highway Officials, and plans shall be submitted for review and approval by the City Engineer.
3. Bridge deck width shall match the roadway approach widths when curb and gutter approaches are provided 26 feet and the face of the bridge rail shall be placed in line with the face of the curb for speed limits less than 45 mph or greater the face of the bridge rail shall be offset 4 feet from the edge of pavement. Curb and gutter will be transitioned out in 50 feet to line up the face of curb and bridge rail. For shoulder section roadways, the bridge width should be a minimum of 24.'

4. The following materials are acceptable for bridge construction:
 - a. For substructures - reinforced concrete, structural steel, treated timbers, prestressed concrete or steel piles or combination of these materials.
 - b. For superstructures - prestressed concrete, structural steel I-beams with reinforced concrete deck or corrugated metal deck with asphaltic wearing surface, treated timber stringer with reinforced concrete deck.
 - c. The type and design of bridge rail shall be as approved by the City Engineer.
5. All materials and workmanship used on construction of the structure shall begin accordance with North Carolina Division of Highways Specifications.

Roadway Dams

1. It is the policy of the City of Fayetteville to discourage the location of roadways on dams. In those cases, where a definite advantage may be gained or a substantial savings in funds may be realized, the utilization of a dam for a roadway may be favorably considered.
2. Where it is determined that a dam will be utilized as a roadway, the following criteria must be met:
 - A. When applicable, the dam must have certification from the N.C. Department of Environment, Health and Natural Resources pursuant to the "Dam Safety Law of 1967," (As amended by the General Assembly of 1977).
 - B. All pertinent data regarding the design of the embankment as an impoundment structure must be presented to the City Engineer for review.
 - C. The top cross section dimension must be the roadway width required (from shoulder point to shoulder point) for the facility plus a minimum of 4.'
 - D. Guardrail will be provided on the impoundment side of the roadway. For safety the Traffic Engineer may require guardrail on both sides of the roadway.

E. Spillway will be designed to provide 2' of freeboard at the shoulder for an estimated 50 year design frequency outflow as a minimum, unless more stringent criteria is required by the DEHNR.

F.A means of draining the lake completely will be provided.

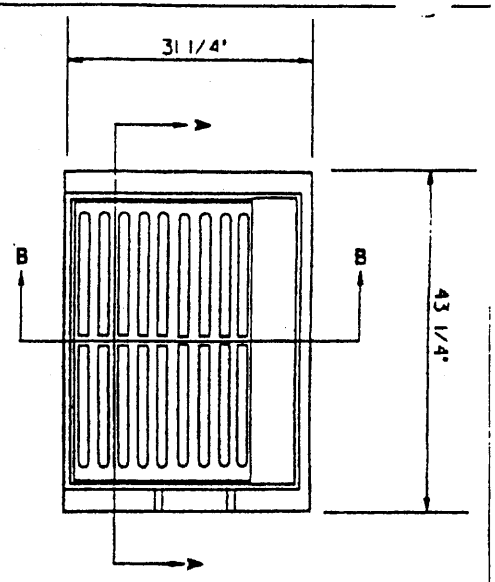
3. Design, acceptance or approval by the City Engineer and the Traffic Engineer is limited to the use of the dam as a roadway and is in no way intended as approval of the embankment as an impoundment structure.
4. Responsibility incurred by the City when a section of roadway crossing a dam is accepted as part of the City maintenance system is limited to maintenance of the roadway for highway purposes from shoulder point to shoulder point only. Responsibility for the impoundment, any damage that may result therefrom, and maintenance of the dam or appurtenance as may be required to preserve its integrity as a water impoundment structure, shall remain with the owner of the impoundment. Any such maintenance work will be subject to the provisions of the City.
5. Impoundment of water on street right-of-way may be allowed under the following criteria:
 - A. The impoundment does not adversely affect the right-of-way for street purpose.
 - B. Adjustment, as required, flattening slopes, installing rip rap, and any others shall be the responsibility of the developer.
6. Structures should be designed and plans prepared under the supervision of a Registered Professional Engineer.

C. CURB AND GUTTER

All curb and gutter sections must meet City of Fayetteville standards.

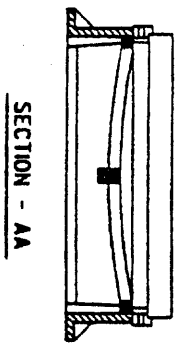
1. The standard 2' rolled concrete curb and gutter is the type to be used for residential streets.
2. The standards 2' 6" 90 degree vertical high back concrete curb and gutter is to be used on all commercial zoned collector streets.
3. Curb inlets shall be standard grate, frame and hoods (Standard 840.3) with curb transition similar to standard 852.05.

NOTE: ALL DIMENSIONS ARE IN FEET AND INCHES UNLESS OTHERWISE INDICATED

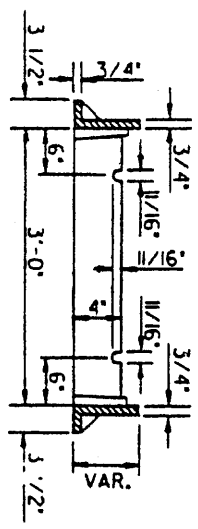


FRAME, GRATE, & HOOD ASS'Y

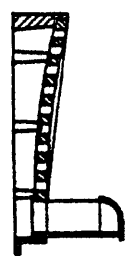
PLAN



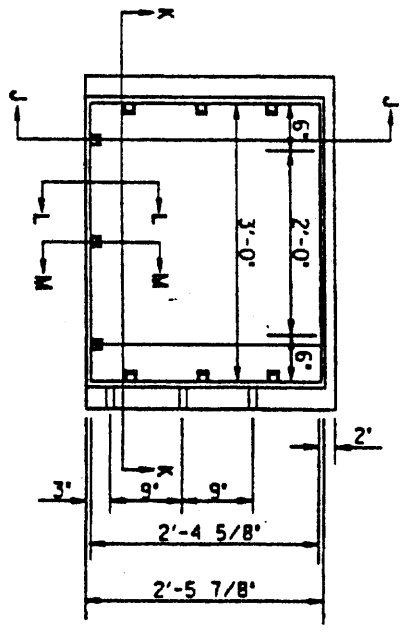
SECTION - AA



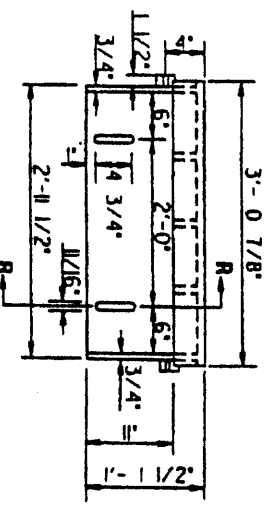
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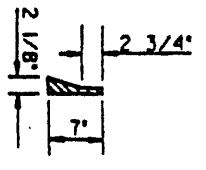
SECTION - BB



FRAME PLAN



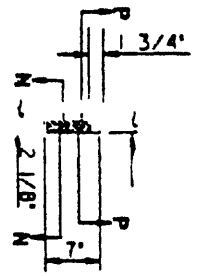
HOOD ELEVATION



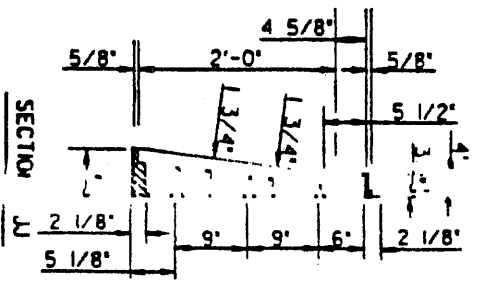
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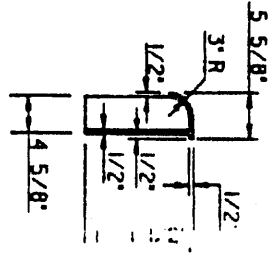
SECTION - PP



SECTION - MM



SECTION - JJ



SECTION - RR

NO.	DATE	DESCRIPTION
5	12-8-88	Revised and Reframed

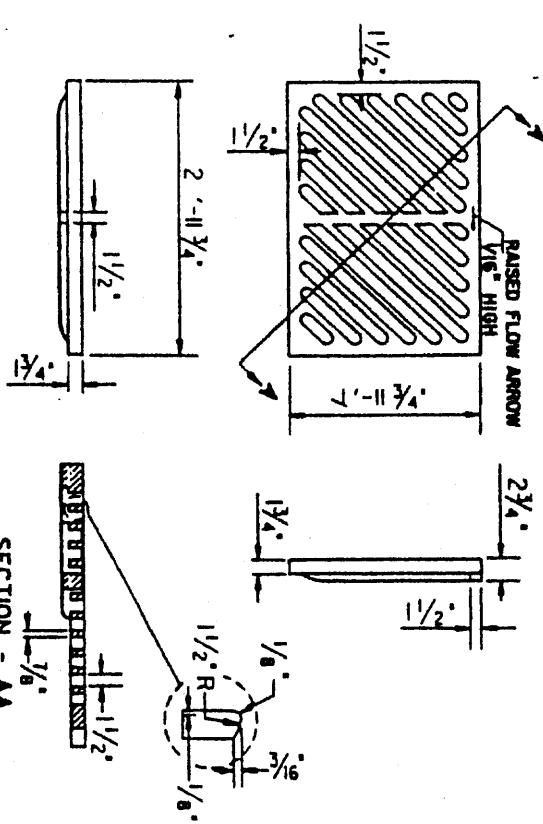
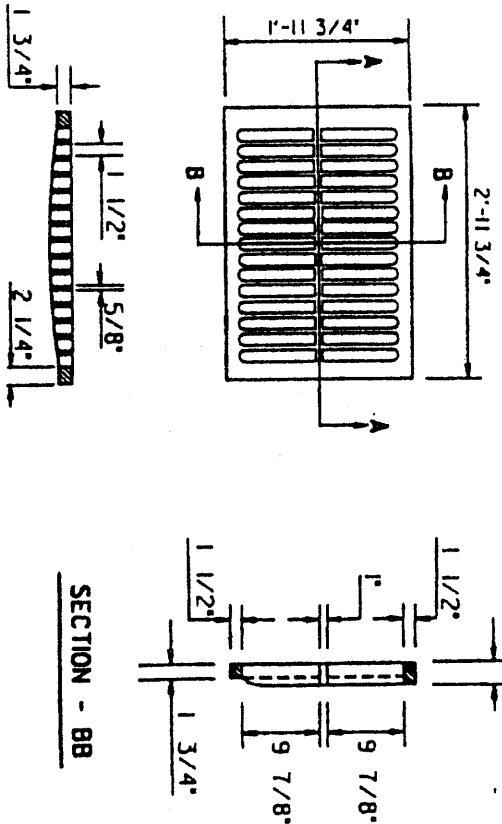
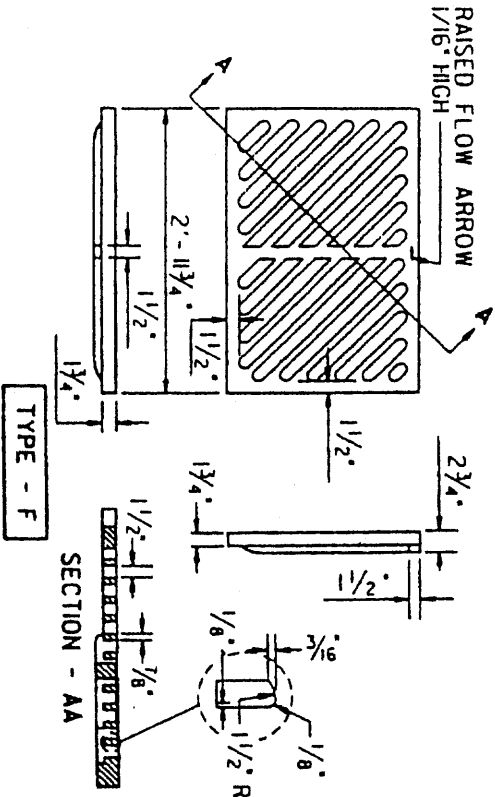
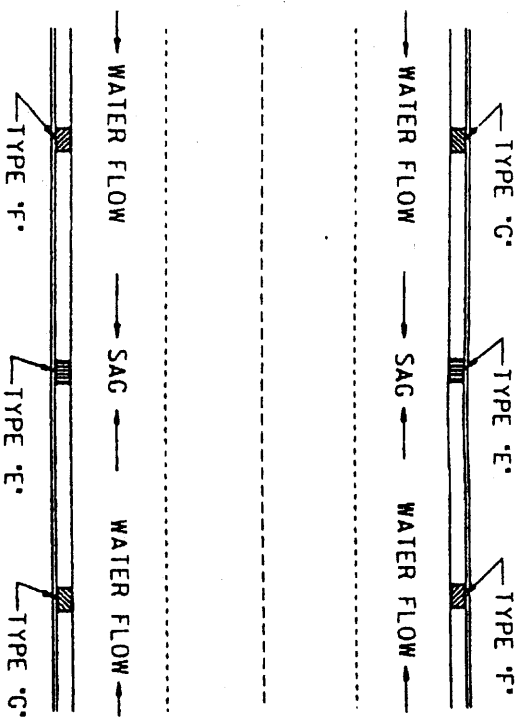
REVISIONS

STANDARD FRAME GRATES AND HOOD

FOR USE ON STANDARD CATCH BASIN
STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS
RALEIGH, NC

ST. 106 3
REV. C

DETAIL SHOWING TYPES OF GRATES TO BE USED ACCORDING TO WATER FLOW.

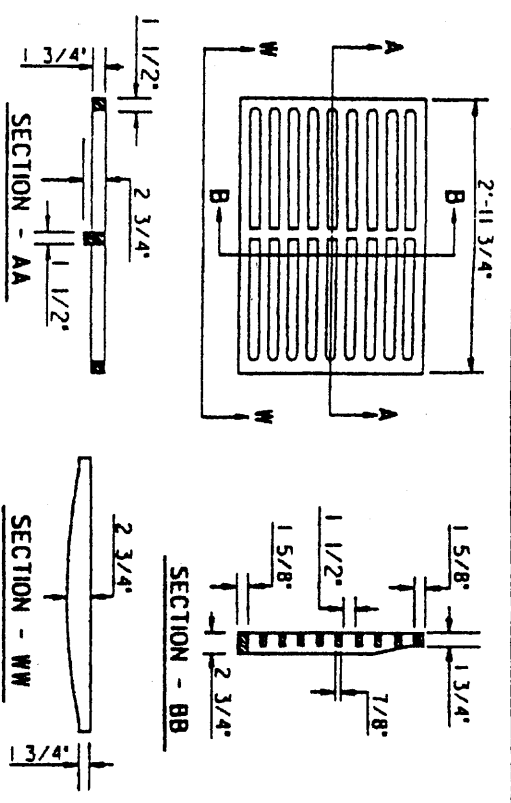


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C	12-19-89	REVISED AND REDRAWN	

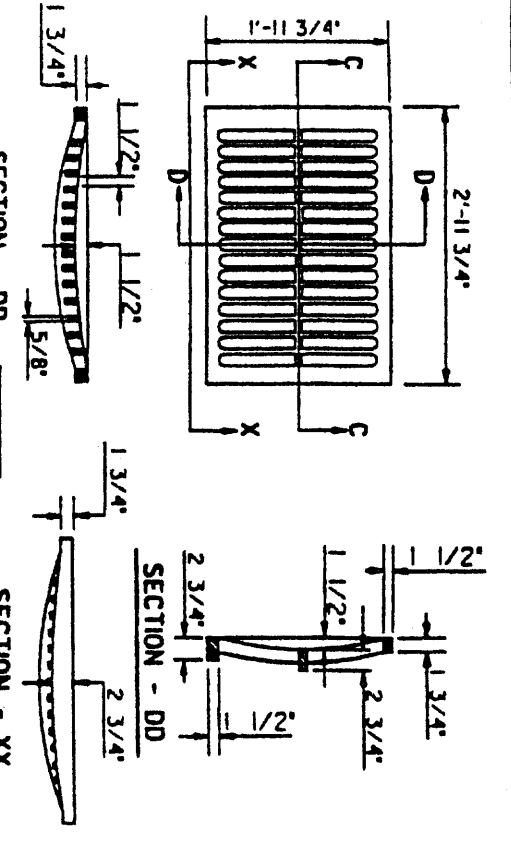
STANDARD FRAME, GRATES, AND HOOD

FOR USE ON STANDARD CATCH BASIN
STAFF, OF PORTLAND CEMENT
DIVISION OF HIGHWAYS
RALEIGH, N.C.

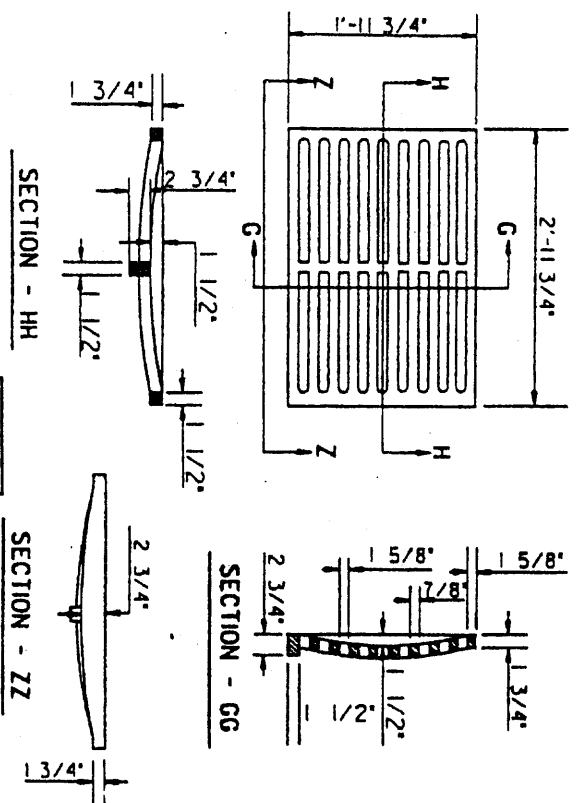
STD. NO. REV.
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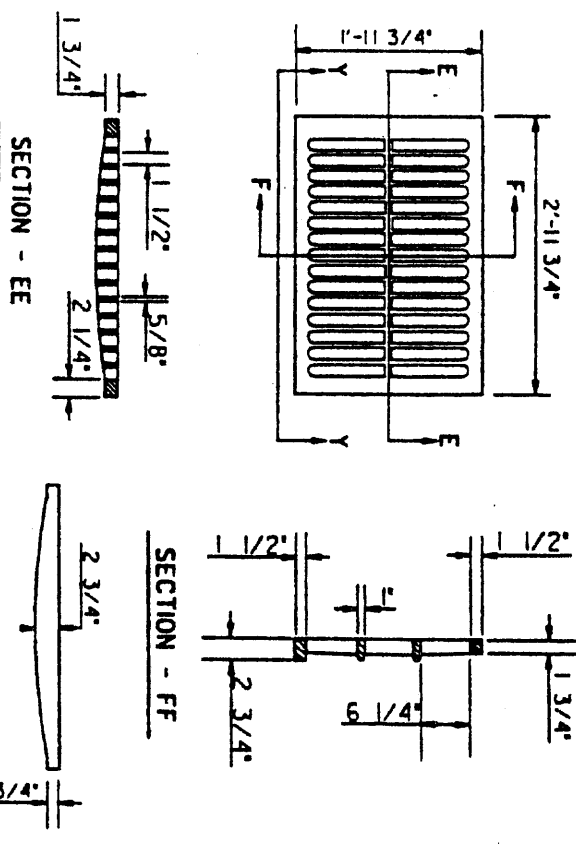
TYPE D



TYPE C



TYPE A

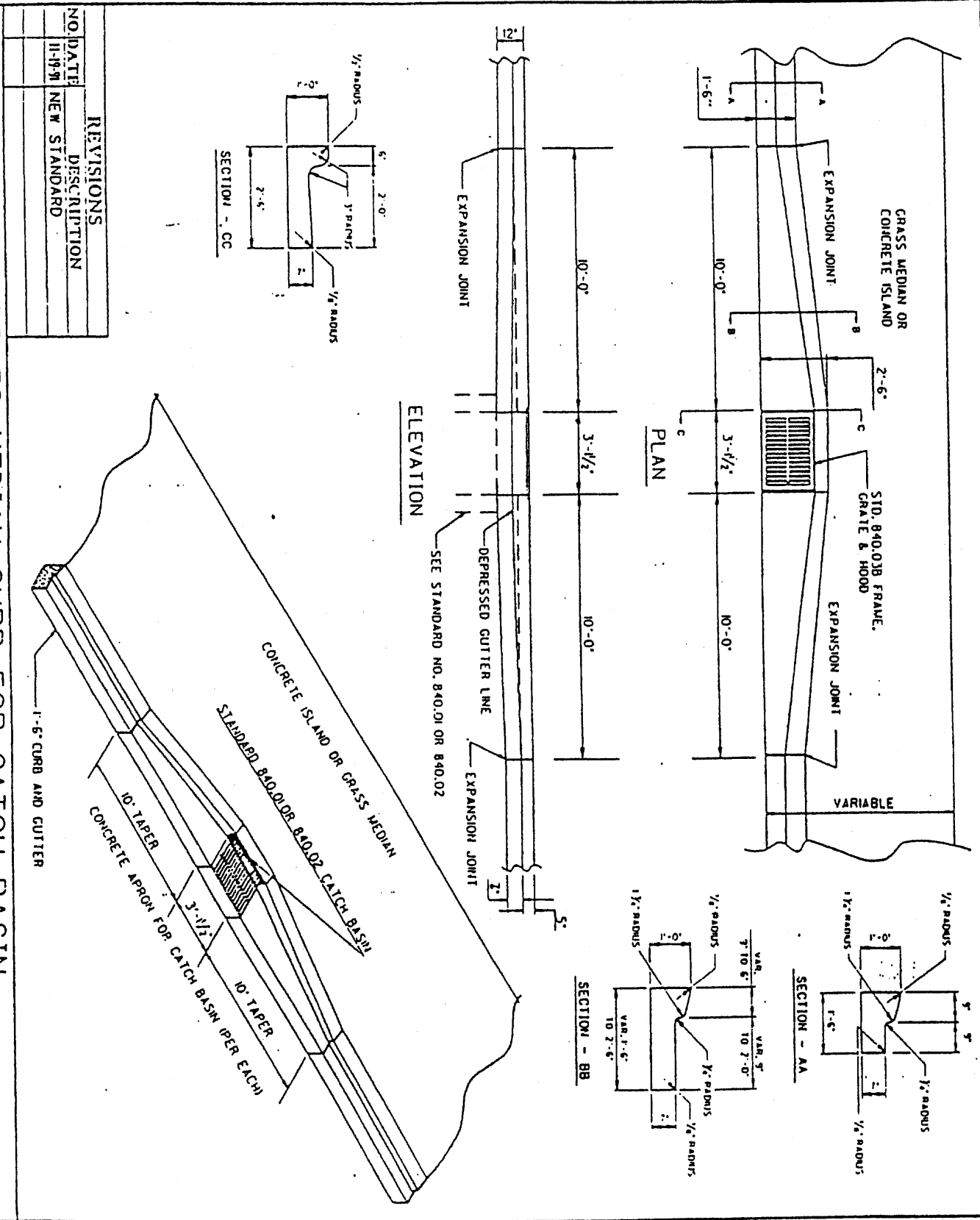


TYPE B

NO.	DATE	REVISIONS	DESCRIPTION
C	12-18-89	Revised and Retitled	

STANDARD FRAME, GRATES, AND HOOD

FOR USE ON STANDARD CATCH BASIN
STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS



REVISIONS	
NO.	DATE
1	11-19-91
	NEW STANDARD
DESCRIPTION	

STANDARD MEDIAN CURB FOR CATCH BASIN

(FOR USE WITH 1'-6" CURB AND CUTTER)
 STATE OF NORTH CAROLINA
 DIVISION OF HIGHWAYS
 RALEIGH, NC

STD. NO.	REV.
852.05	

4. Curb inlets will be spaced a maximum of 500 feet apart and more frequently on moderately steep gradients where curb overtopping might occur.

D. WHEEL CHAIR RAMPS

In accordance with General Statute 136-44-.14, all street curbs in North Carolina being constructed or reconstructed for maintenance procedures, traffic operations, repairs, correction of utilities or altered for any reason after September 1, 1973, shall provide wheel chair ramps for the physically handicapped at all intersections where curb and gutter is provided and at other major points of pedestrian flow.

Wheel chair ramps and depressed curbs shall be constructed in accordance with guidelines set forth by the American Disabilities Act.

E. PAVEMENT DESIGNS

Shown below are minimum thicknesses of base and surface course to be used. Design should be chosen from Group 1 or Group 2 depending on subgrade soil type.

GROUP I

Good to Excellent Subgrade Soil Types	Base Course	Pavement Surface
A-1-a, A-1-b, A-3	7" STBC, Type A or C	2" SA or I-2
A-2-4, A-2-5, A-2-6, A-2-7	9" STBC, Type A or C	1 1/2" SA or I-2
	8" ABC or STBC, Type B	BST, 1" SA or I-2
	6" ABC or STBC, Type B	1 1/2" SA or I-2
	3" BCBC, Type HB	1 1/2" SA or I-2
	3 1/2" BCBC, Type HB	1" SA or I-2
		5" Plain Concrete

GROUP II

Poor to Fair Subgrade Soil Types	Base Course	Pavement Surface
A-4, A-5, A-6, A-7-5 A-7-6	9" STBC, Type A or C	2" SA or I-2
	8" ABC or STBC, Type B	1 1/2" SA or I-2
	10" ABC or STBC, Type B	BST, 1" SA or I-2
	4" BCBC, Type HB	2" SA or I-2
	3" BCBC, Type HB	6" Plain Concrete

Any other pavement design must be reviewed by the City Engineer on an individual basis and approval will be based upon sound engineering principles.

NOTE:

Subgrade - No base course shall be placed on muck, pipe clay, organic matter or other unsuitable material. The City Engineer may require a subgrade soils test, if needed to determine the soils classification type.

- ABC** - Aggregate Base Course, No. 7 stone
- STBC** - Soil Type Base Course
- BST** - Bituminous Surface Treatment
- SA** - Bituminous Concrete Surface Course Type F-1 (Sand Asphalt)
- I-2** - Bituminous Concrete Surface Course, Type I-2
- BCBC** - Bituminous concrete Base Course, Type HB (Black Base)

Other base courses such as various cement-treated materials may be used in lieu of those shown above. These materials shall be of sufficient thickness to provide equivalent strength. However, any design other than those shown above must also be approved prior to use by the City Engineer.

All materials shall meet the requirements set forth in the latest edition of the North Carolina Standard Specifications for Roads and Structures.

F. MINIMUM DESIGN CRITERIA

1. Local Residential Street

- a. Cul-de-Sacs
- b. Dead End Roads - Less than 1000 feet in length
- c. Short Connecting Roads - One block long or that extend on a block-by-block basis
- d. Loop Roads - Less than one (1) mile in length
- e. Other roads that do not connect thoroughfares or serve major traffic generators. These Roads do not have "collector" characteristics.

TERRAIN CLASSIFICATION

LEVEL

ROLLING

HILLY

Terrain Classification Definition

Level - Cross slope range of 0% to 8%

Rolling - Cross slope range of 8.1% to 15%

Hilly - Cross slope over 15%

Right-of-Way Width

Curb and Gutter Section	40'	40'	40'
Shoulder Section	45'	45'	45'

B - C refers to back of curb of standard curb and gutter section.

Pavement Width

Curb and Gutter Section 27' B - C 27' B - C 27' B - C

Minimum Shoulder Width

Shoulder Section 6'4' to 6'4' to 6'

Shoulder Width graded 1/4" to 1/2" per foot up or down from back/curb or edge of pavement to right-of-way line shall be approved by the City Engineer considering adjacent land characteristics.

Maximum Cut and Fill Slopes	2:1	2:1	1 1/2:1
Design Speed	30 mph	25 mph	20 mph
Minimum Sight Distance on Vertical Curves	200'	150'	110'
Minimum Centerline Radius	230'	150'	90'

Maximum Grade

Grades for 100' each way from intersections exceeding 5 percent (%) may be reviewed by City Engineer for consideration. Grades less than 0.5 percent should not be used unless reviewed individually by the City Engineer to determine potential maintenance problems.

**K=Rate of Vertical Curvature
for minimum Sight Distance**

Formula for determination of length of vertical curve required to provide minimum sight distance.

- [L = KA]
- L = Length of vertical curve in feet
- K = Rate of vertical curvature in feet per percent of A
- A = Algebraic difference in grades in percent

28 18 10

Minimum Cul-De-Sac Radius

Right-of-Way

Curb and Gutter Section	45'	45'	45'
Shoulder Section	50'	50'	50'

Minimum Cul-De-Sac Radius

Curb and Gutter Section	37'	G37'	G37'	G
Shoulder Section	35'	35'	35'	

2. Residential Collector Roads include:

- a. Dead End Roads - Less than 2000 feet in length
- b. Connecting roads between the local residential subdivisions roads and the thoroughfare system
- c. Loop Roads - More than one (1) mile in length
- d. Other roads having a "collector" type function in the thoroughfare system

TERRAIN CLASSIFICATION LEVEL ROLLING HILLY

Terrain Classification Definition

Level - Cross slope range of 0% to 8%

Rolling - Cross slope range of 8.1% to 15%

Hilly - Cross slope over 15%

Right-of-Way Width

Curb and Gutter Section	50'	50'	50'
Shoulder Section	50'	50'	50'

Pavement Width

Curb and Gutter Section	35'	B-C35'	B-C35'	B-C
Shoulder Section	20'	20'	20'	

B - C refers to back of standard curb and gutter section. Other types will be reviewed by the City Engineer with the B - C limits applicable.

Minimum Shoulder Width

Shoulder Section 6-6-6	6'	6'	6'
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Maximum Cut and Fill Slopes	2:1 2:1	2:1	1 1/2:1
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Design Speed	35 mph	30 mph	25 mph
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Minimum Site Distances on Vertical Curves	250'	200'	150'
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Minimum Centerline Radius	310'	230'	150'
Minimum Superelevation Rate for Minimum Radius	.08'/'	.06'/'	.04'/'

Maximum Grade

Grades for 100' each way from intersection exceeding 5 percent (%) may be reviewed by City Engineer for consideration. grades less than 0.5 percent should not be used unless reviewed individually by the City Engineer to determine potential maintenance problems.

K = Rate of Vertical Curvature
for minimum site distance

Formula for determination of length of vertical curve required to provide minimum
site distance.

45

28

18

$$[L = KA]$$

L = Length of vertical curve in feet

K = Rate of vertical curvature in feet per percent of A

A = Algebraic difference in grades in percent

3. Thoroughfare Plan Roads

For subdivision resulting in the construction of a thoroughfare plan route by the developer, the subdivision plan will be forwarded to the City Engineer and the traffic Engineer for review.

4. Industrial Access or Commercial Complex roads

The minimum construction standards for industrial access road requests or for commercial center and apartment complexes will be reviewed individually. The construction standards for pavement design will be in line with expected usage. Normally, you can expect roads of this type to have a pavement design of

1. 8" ABC - 2" BCBC Type H plus 2" BCSC Type I-2 or I-1
2. 8" ABC - 2" BCBC Type I-2 or I-1

G. ROAD INTERSECTION (See Figures 3 and 4)

1. The most desirable intersections are those with angles of 75 to 90 degrees. Intersections with angles for 60 to 75 degrees are acceptable under extreme conditions.

2. Minimum sight distance for stop condition when connecting new local residential roads or residential collector roads to existing state maintained roads is 70 feet along the existing road right-of-way and 10 feet along the new road right-of-way.
3. All internal intersection shall have a minimum 25' curb radii.
4. Unusual designs such as "Bubble Type" configurations to allow for more lots, will need prior approval by City Engineer.

H. ISLANDS OR SHORT MEDIANS AT SUBDIVISION ENTRANCES

The City of Fayetteville will review requests for the allowance of islands or short medians desired for aesthetics on City Streets and at the entrance to a subdivision. The location will be outside the line of sight at the entrance intersection. Approval will be with the following understanding:

1. The City will not maintain the island or the median section.
2. The island or the median section will be removed if not properly maintained by someone involved with the subdivision, i.e.
3. The City Engineer may allow the island or median sections after review on an individual basis.
4. The minimum lane width at the entrances, excluding curb and gutter will be 20'.

I. SUBDIVISION NAME MAKERS

The City will review requests to erect subdivision name markers on an individual basis. The name markers may be allowed to be located within the City rights of way at the beginning of a subdivision road provided the location of such is outside the line of sign and the normal maintenance limits. The name markers will be approved only at locations which will not sacrifice safety to the general traveling public. Approval to erect subdivision name markers will be with the following understanding.

1. All costs will be the responsibility of the requestor.
2. The City will not maintain the marker or the area around the marker.

3. The markers will be removed if not properly maintained.
4. The City may allow the subdivision name markers on secondary road rights of way after on an individual basis.

Cul-de-sac designs or T-Turn Arounds will be subject to the approval of the City Engineer after review on an individual basis.

Other base courses such as various cement-treated materials may be used in lieu of those shown above. These materials shall be of sufficient thickness to provide equivalent strength. However, any design other than those shown above must also be approved prior to use by the City Engineer.

All materials shall meet the requirements set forth in the latest edition of the North Carolina Standard Specifications for Roads and Structures.

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF FAYETTEVILLE
AMENDING CHAPTER 27 OF THE CODE OF ORDINANCES OF THE CITY OF
FAYETTEVILLE TO ESTABLISH A PRIVATE STREET STANDARD

BE IT ORDAINED by the City Council of the City of Fayetteville, North Carolina, that Chapter 27 of the Code of Ordinances, City of Fayetteville, North Carolina, is hereby amended as follows:

Section 1. A new Section 27-1(ee), Private street, is created as follows:

(ee) *Private street.* A vehicular way, with a street name approved by the City of Fayetteville; maintained by a private entity (individual, corporation, or homeowner's association); properly platted for individual dwelling unit ownership; certified; and recorded with required disclosures in accordance with Section IV, Private Streets.

Section 2. A new Section 27-11, Private streets, is created as follows:

Sec. 27-11. Private streets.

Private streets will be permitted to serve as access within developments with individual dwelling unit ownership; however, reservation of corridors for thoroughfares and utilities may be required if such are indicated on official plans as adopted by the City Council of the City of Fayetteville. Private streets may be used in residential developments to provide for security and privacy. A variety of construction materials may be used if approved by the City Engineer.

Properties adjacent to private streets may be deeded to the center of the street with the reservation of the required street easements or may be deeded to the margin of the designated private street right-of-way. All areas not deeded with individual properties will be a part of the Common Area and will be owned and maintained by the homeowner's association. Setbacks required for the zone in which the property is located will be measured from the margin of the right-of-way or street easements, unless the development is designated as "Zero Lot Line", in which case the requirements for "Zero Lot Line" will be used.

In residential developments, where properties are intended for sale to individuals, a right-of-way or easement shall be designated and shown on the record plat. Minimum width for right-of-ways or easements shall be the width of the paved section and an additional eight (8) feet on each side.

- (a) *Maintenance.* All subdivisions and residential developments in which properties are intended to be sold and which contain private streets shall create a homeowner's association for maintenance of such streets. The homeowner's documents must provide for adequate enforcement and funding to insure maintenance, and the documents must be approved as to form by the City Attorney.

In developments that are to be retained under single ownership and units or lots will be for rental purposes only, maintenance of private streets shall be provided, the same as for open space or other facilities for the purpose of common use.

- (b) *Access to government agency.* In any development where private streets are provided, a plat shall be prepared for Planning Department approval and recorded in the Office of the Cumberland County Register of Deeds, the plat of such development indicating all private streets. It shall be indicated on such plat that any governmental agency or personnel or equipment thereof shall be granted perpetual access over all such private streets to accomplish or fulfill any service or function for which the agency is responsible, and that any agency or organization designated by a governmental agency to perform a designated function shall also retain access the same as any governmental agency. It shall be indicated on such plat that any agency exercising its access rights shall have the same rights and only such liabilities as it would have on any public lands, rights-of-way, or easements.

- (c) *Design.*

- (1) *Width.* All private streets and drives shall have a minimum width of travel way of twenty (20) feet.
- (2) *Driveway surfacing.* A variety of surfacing and materials will be permitted for the construction of private streets;

however, in all cases, the design shall be such that the load bearing capacity and wearing surface shall be equal to or exceed that of a City of Fayetteville "Residential Street".

- (3) *Corner turning radii.* Minimum twenty-five (25) feet.
 - (4) *Cul-de-sac.* Where looped streets are not provided, a cul-de-sac or "T" backing space shall be provided at the ends of all dead-end streets.
 - (5) *Parking bays.* Parking bays are permitted on private streets, however, the bays shall be designed so that they do not encroach into the horizontal clearance required in (c)(1) above.
- (d) *Certification of construction.* Upon completion of construction of private streets and related facilities, including drainage systems, the developer shall provide for an inspection of all such facilities by a Registered Engineer, who shall provide in writing a statement that all private streets and related facilities are constructed in accordance with the requirement of this ordinance and the approved plans. Such statement shall be affixed with the engineer's seal and submitted to the Planning Board and approved prior to the recording of a final plat.
- (e) *Signage.* Internal direction signs, stop signs, and other signage may vary from the standard City of Fayetteville signs, except for public street entrances, which must be in conformance with size, shape, color, and Reflectivity Requirements of the Manual on Uniform Traffic Control Devices (N.C.G.S. § 136-30).
- (f) *Disclosures.*
- (1) Every plat approved herein for residential development containing private streets shall contain the following statement printed thereon: Street rights-of-way shown on this plat are for private use, have not been accepted for maintenance by the Fayetteville City Council, it is City Council policy not to accept streets that

do not meet public street standards, and are to be maintained by _____ (in the case where a homeowner's association owns the streets, homeowner's association shall be typed in the blank; in the case where lots are deeded to the center of the private street, the term "lot owner" shall be typed in the blank).

- (2) The approval sheet on any preliminary or final plat approved by either the Planning Board or its staff, shall contain the following statements:
 - a. Prior to recording, the disclosure statement as set forth in Section 27-11(f)(1) of the Code of Ordinances of the City of Fayetteville shall be placed on the plat; and
 - b. Every deed conveying a lot shown on the plat approved herein shall contain the following statement: It is City Council policy not to accept streets that do not meet public street standards. The street upon which the property described herein abuts is a private street, the cost of maintenance which shall be born by either the grantee herein or a homeowner's association as set forth in any restrictive covenants applicable to this conveyance and recorded in Book _____, Page _____, of the Cumberland County Registry, which shall be a covenant to run with the land.
- (3) Any declaration of conditions and/or restrictive covenants required to be recorded by this Chapter 27, shall contain provisions setting forth the ownership, and responsibility for maintenance of any private streets within the property subject to said declaration of conditions and restrictive covenants.

Section 2. It is the intention of the City Council, and it is hereby ordained that the provisions of this ordinance shall become and be made part of the Code of Ordinances, City of Fayetteville,

North Carolina, and the sections of this ordinance may be renumbered to accomplish such intention.

ADOPTED this the 21st day of March, 1994.

CITY OF FAYETTEVILLE

By: J. L. Dawkins
J. L. Dawkins, Mayor

ATTEST:

Janet C. Jones
Janet C. Jones, Deputy City Clerk

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF FAYETTEVILLE
AMENDING SECTION 32-22 (9), R15 RESIDENTIAL DISTRICT, OF THE
CODE OF ORDINANCES OF THE CITY OF FAYETTEVILLE

BE IT ORDAINED by the City Council of the City of Fayetteville,
that:

Section 1. Section 32-22 (9), R15 Residential District, of the Code of Ordinances of the City of Fayetteville, North Carolina, is amended by changing the period at the end thereof to a comma, and adding the following words, to wit: including retail sales and concessions of food, beverages, and merchandise related to the activities such as clothing and equipment, as approved by the public body.

Section 2. It is the intention of the City Council, and it is hereby ordained that the provisions of this ordinance shall become and be made part of the Code of Ordinances, City of Fayetteville, North Carolina, and the sections of this ordinance may be renumbered to accomplish such intention.

Adopted this the 31st day of March, 1994.

CITY OF FAYETTEVILLE

J. L. Dawkins
J. L. Dawkins, Mayor

Attest:

Janet C. Jones
J. Deputy City Clerk